

SH249 Jon C Fultz Residential Access

+/- 10,567' west of FM1774
GPS 30 17 54.59N 95 52 4.08W

Legend



FM249 Jon C Fultz 2043+00

SH249 Jon C Fultz Parcel 2043+00 Residential



100 ft



Permit to Construct Access Driveway Facilities on Highway Right of Way

PERMIT NUMBER: TxDOT ENTER PERMIT NUMBER HERE			
REQUESTOR		GPS*	ROADWAY
		LATITUDE, LONGITUDE	HWY NAME
		30.2985N 95.8678W	SH249
		FOR TxDOT'S USE	
NAME	Jon C Fultz	CONTROL	4-DIGIT
MAILING ADDRESS	P. O. Box 868	SECTION	2-DIGIT EX. 01
CITY, STATE, ZIP	Navasota, Texas 77868		
PHONE NUMBER	936-825-7833		
<small>*GLOBAL POSITIONING SYSTEM COORDINATES AT INTERSECTION OF DRIVEWAY CENTERLINE WITH ABUTTING ROADWAY</small>			

Is this parcel in current litigation with the State of Texas? YES NO

The Texas Department of Transportation, hereinafter called the State, hereby authorizes Jon C Fultz, hereinafter called the Permittee, to construct / reconstruct a residential (residential, convenience store, retail mall, farm, etc.) access driveway on the highway right of way abutting highway number SH249 in Grimes County, located (+/-) 10,567 west of FM1774.

USE ADDITIONAL SHEETS AS NEEDED

This permit is subject to the Access Driveway Policy described on page 2 and the following:

1. The undersigned hereby agrees to comply with the terms and conditions set forth in this permit for construction and maintenance of an access driveway on the state highway right of way.
2. The Permittee represents that the design of the facilities, as shown in the attached sketch, is in accordance with the Roadway Design Manual, Hydraulic Design Manual and the access management standards set forth in the Access Management Manual (except as otherwise permitted by an approved variance).
3. Construction of the driveway shall be in accordance with the attached design sketch, and is subject to inspection and approval by the State.
4. Maintenance of facilities constructed hereunder shall be the responsibility of the Permittee, and the State reserves the right to require any changes, maintenance or repairs as may be necessary to provide protection of life or property on or adjacent to the highway. Changes in design will be made only with prior written approval of the State.
5. The Permittee shall hold harmless the State and its duly appointed agents and employees against any action for personal injury or property damage related to the driveway permitted hereunder.
6. Except for regulatory and guide signs at county roads and city streets, the Permittee shall not erect any sign on or extending over any portion of the highway right of way. The Permittee shall ensure that any vehicle service fixtures such as fuel pumps, vendor stands, or tanks shall be located at least 12 feet from the right of way line to ensure that any vehicle services from these fixtures will be off the highway right of way.
7. The State reserves the right to require a new access driveway permit in the event of: (i) a material change in land use, driveway traffic volume or vehicle types using the driveway, or (ii) reconstruction or other modification of the highway facility by the State.
8. The State may revoke this permit upon violation of any provision of this permit by the Permittee.
9. This permit will become null and void if the above-referenced driveway facilities are not constructed within six (6) months from the issuance date of this permit.
10. The Permittee will contact the State's representative Justin Kalisek at TXDOT Grimes County Maintenance Office at telephone, (936) 825-3446, at least twenty-four (24) hours prior to beginning the work authorized by this permit.
11. The requesting Permittee will be provided instructions on the appeal process if this permit request is denied by the State.

The undersigned hereby agrees to comply with the terms and conditions set forth in this permit for construction and maintenance of an access driveway on the highway right of way.

Date: 4/20/2022

Signed: Jon C Fultz Jon C Fultz
PCARE7DF730E449... (Property owner or owner's representative)

<u>4/20/2022</u>	<u>James W Robbins</u> <u>James Robbins, P.E.</u> <small>DBE1B2AE6E93449... (District Engineer, or designee Approval)</small>
Date of Issuance	District Engineer, or designee Approval
Date of Issuance as per Variance to AMM	District Engineer, or designee Approval
Date of Denial	District Engineer Denial (No Delegation)

Access Driveway Policy


Title 43 Texas Administrative Code (Transportation), Chapter 11 (Design), Subchapter C (Access Connections To State Highways) and the "Access Management Manual" establish policy for the granting of access and the design, materials, and construction of driveways connecting to state highways. All driveway facilities must follow this policy. To the extent there is any conflict between this permit and the policy, the policy shall control. If a proposed driveway does not comply with the access management standards, the owner may seek a variance to a requirement contained in the access management standards by contacting the local TxDOT office.

TxDOT Driveway Permit Request Contact

For a local contact for your TxDOT Driveway Permit Request or variance request, visit: <http://www.txdot.gov/inside-txdot/district.html>. You can click on the section of the map closest to your location to find the local TxDOT office. You can also click on the drop down box below the map to find the district for your county.

Other Conditions

In addition to Items 1 thru 11 on page 1 of this permit, the facility shall also be in accordance with the attached sketch and subject to the following additional conditions stated below:

SH249 Jon C Fultz Residential Access Driveway See Attached Provisions (Important)	
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Variance Documentation Justification

For a Variance request, please indicate which of the below are applicable, as required by TAC §11.52(e):

- a significant negative impact to the owner's real property or its use will likely result from the denial of its request for the variance, including the loss of reasonable access to the property or undue hardship on a business located on the property.
- an unusual condition affecting the property exists that was not caused by the property owner and justifies the request for the variance.

For the conditions selected above, provide written justification below. (Attach additional sheets, if needed)

For TXDOT use below:

For Variance denials, please indicate which of the below conditions, as provided in TAC §11.52(e), were determined:

- adversely affect the safety, design, construction, mobility, efficient operation, or maintenance of the highway; or
- likely impair the ability of the state or the department to receive funds for highway construction or maintenance from the federal government.

Attachments:

Sketch of Installation

All Variance Documentation

April 20, 2022

SH 249 (North ROW)
(+/-)10,567' West of FM1774
Parcel 303 Fultz 2043+00 Rt
Access Driveway Permit

Access Drive Width 20' (25'R) -
2-30" RCP/CMP/HDPE w/6:1 SETS

DS
JCF

GENERAL SPECIAL PROVISIONS (Access Driveway Permit):

- 1) Traffic Control plan is required per TMUTCD and TXDOT standards prior to start of construction in the SH 249 right-of-ways (unless constructed prior to opening SH 249 to public traffic).
- 2) Areas within ROW disturbed by construction shall be restored to equal or better conditions than existing prior to construction.
- 3) Contractor shall be responsible for locating all utilities within the right-of-way construction site. Contacts with local utility companies and utility locators are required.
- 4) **Any lane closure will be allowed only between 8:30 AM and 4:30 PM, qualified flagmen will be required** (unless constructed prior to opening of SH 249 to public traffic).
- 5) No undercutting of existing asphalt pavement will be allowed
- 6) Driveway shall be constructed as per TxDOT Standard Details & Specifications. Driveway radius shall be a minimum of 25'. Pavement shall be Non-trackable.
- 7) 6:1 SET's will be required (See Attached).
- 8) Ditch Sections @ Proposed access driveway shall be graded to drain.
- 9) Pavement surfaces shall be kept clear of mud, debris, etc.
- 10) Construction of access driveway shall be completed within 45 days from the start of construction unless an extension is granted by TxDOT.
- 11) Contractor shall contact Mr. Ronnie Becker at (979)-229-8074 or ronnie.becker@txdot.gov, 1 week prior to starting work.

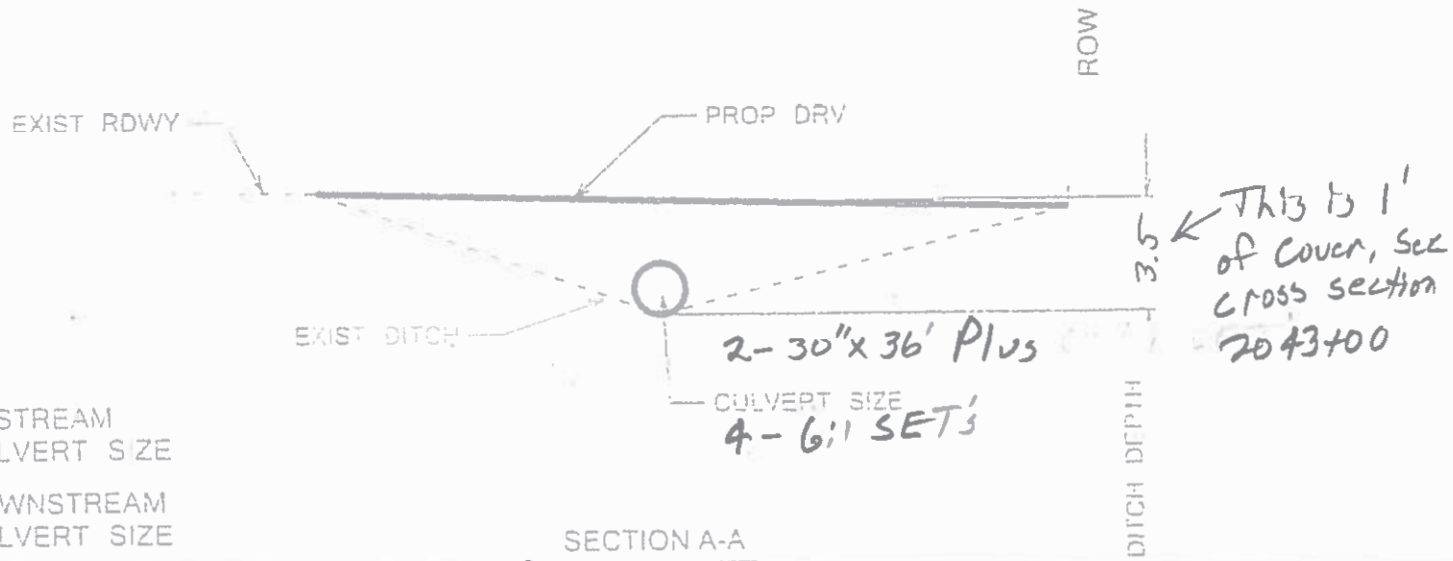
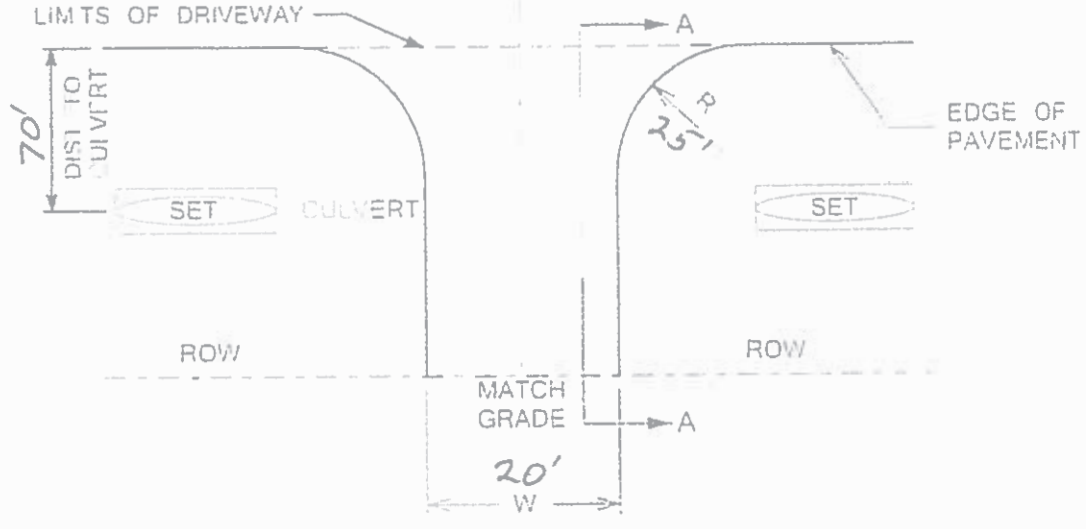
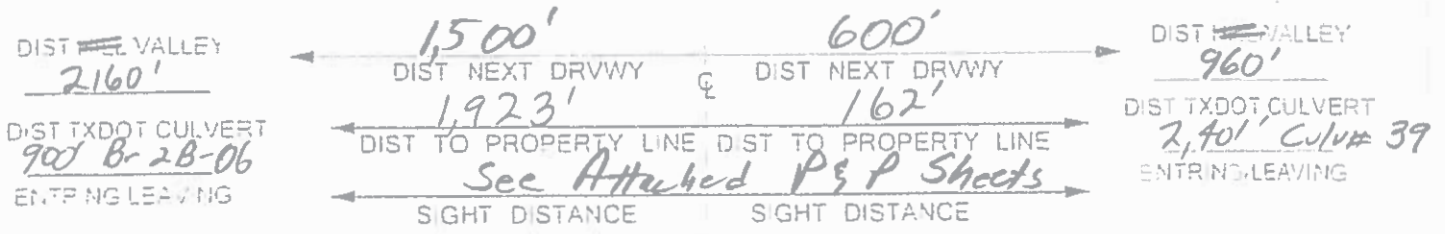
Special Notes: This is a driveway access onto the proposed TXDOT SH249 project.

This permit will be valid for a period of 1 year from the date of approval. If construction of the driveway is not completed in the approved timeframe, requested extensions will be granted in 6-month intervals while the existing ownership and property use has not changed.

Driveway construction should not start until SH 249 contractor has completed all highway construction.

PROPOSED DRIVEWAY PERMIT WORKSHEET

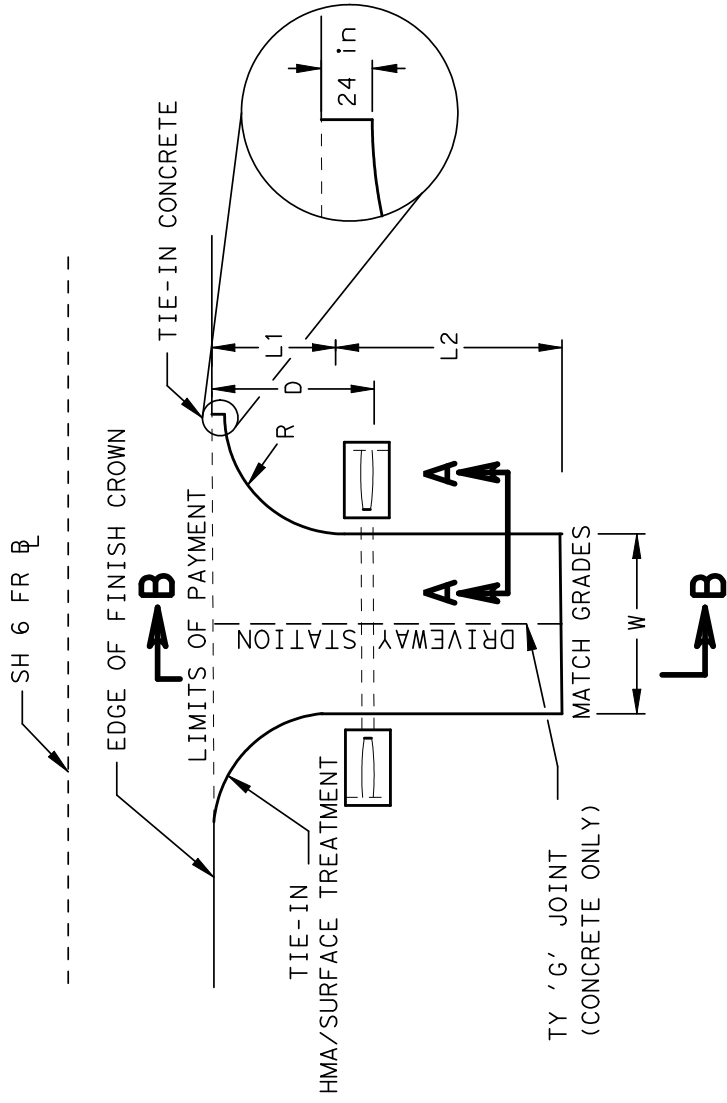
HIGHWAY SH 249 SPEED LIMIT 65 DATE 3-23-22
 LOCATION: RM 426+1,557 LAT LONG 30.2985 N, 95.8678 W
 DRIVEWAY DIMENSIONS: W = 20' L = 137' R = 25'



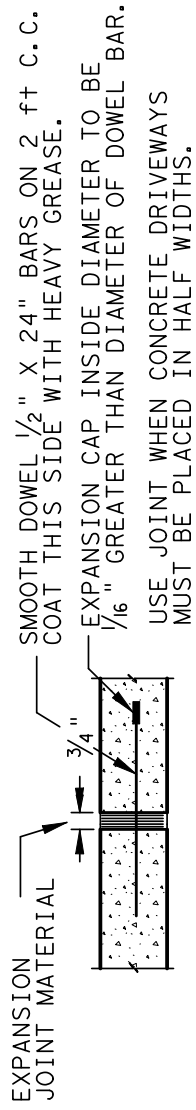
← This is 1' of cover, see cross section 2043+00

$20' + 6(3.5 - 2.5) \times 2 = 32' \Rightarrow 36' \text{ Culu Length}$

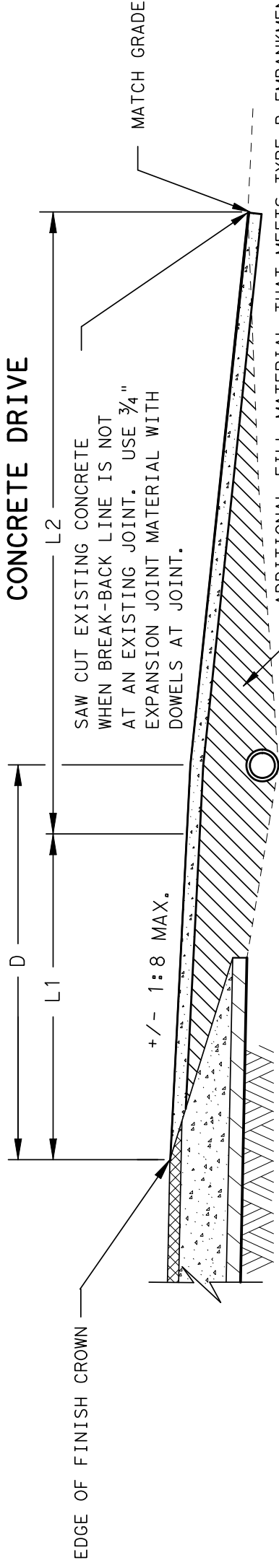
MIN LENGTH CULVERT = DRV WIDTH - 6x(DITCH DEPTH - CULVERT SIZE) x 2
 EX. 24 - 6x(2.5 - 1.5) x 2 = 36'



TYPICAL PLAN VIEW

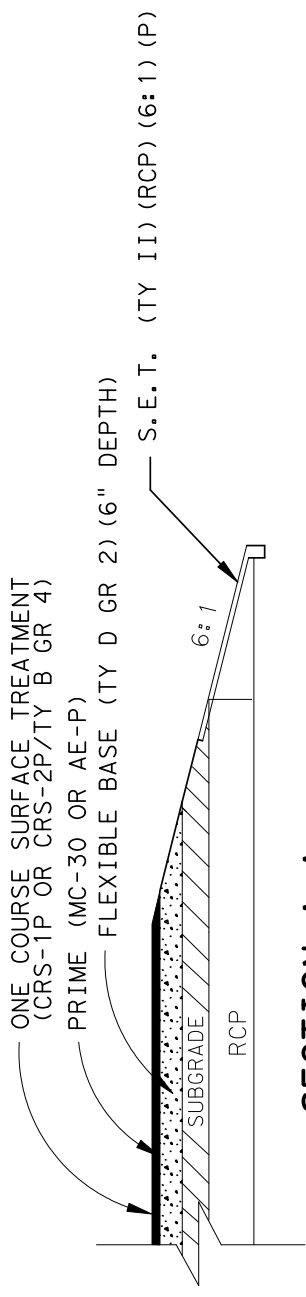


TY 'G' JOINT

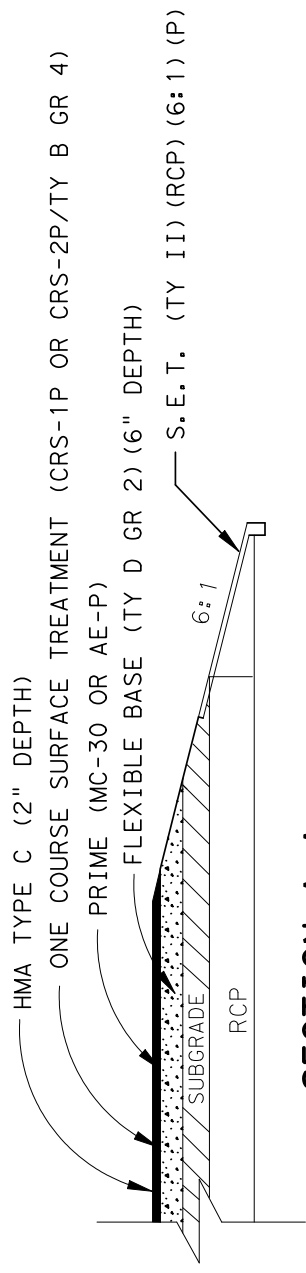


SECTION B-B

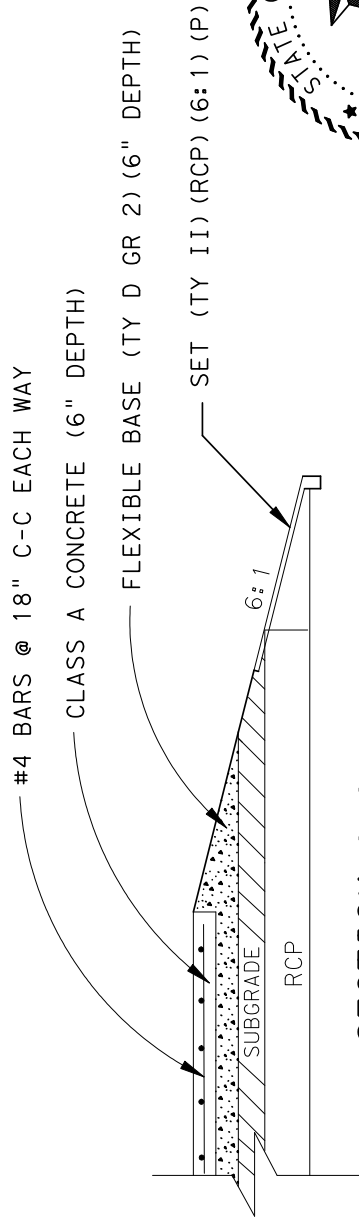
NOTE: LETTER OF PERMISSION NEEDS TO BE OBTAINED FROM PROPERTY OWNERS FOR THE CONSTRUCTION OF DRIVEWAYS THAT EXTEND ONTO PRIVATE PROPERTY. THE CONTRACTOR SHALL CONTACT EACH PROPERTY OWNER PRIOR TO CONSTRUCTION OF THESE DRIVEWAYS.



**SECTION A-A
SURFACE TREATMENT DRIVE**



**SECTION A-A
ASPHALTIC-CONCRETE DRIVE**

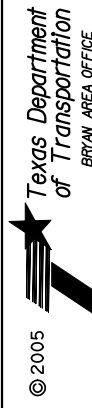


**SECTION A-A
CONCRETE DRIVE**



STANDARDS: SETP-PD, PSET-SP, PSET-RP, OR PSET-RR

N. T. S.

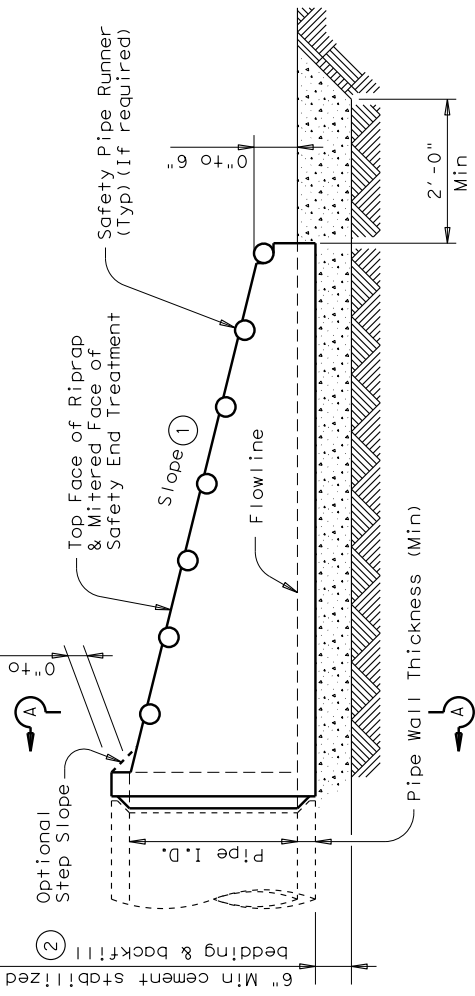


**DRIVEWAY
DETAILS**

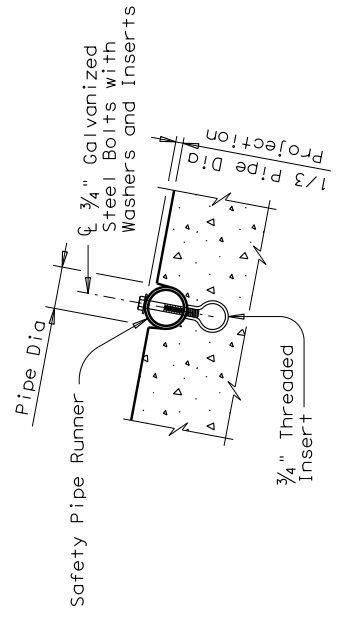
FED. RD. DIV. NO.	PROJECT NO.	SHEET NO.
6	NH 2005(182)	382
STATE	DIST.	COUNTY
TEXAS	BRYAN	BRAZOS
CONT.	SECT.	JOB
0050	02	055.ETC.
		SH 6

PIPE I.D.	MINIMUM WALL THICKNESS	MINIMUM O.D.	MIN O.D. AT TAPERED END	MIN REINF REQUIREMENTS (Sq. in./ft. of pipe)	MAXIMUM SLOPE	MINIMUM LENGTH OF UNIT	PIPE RUNNERS REQUIRED		REQUIRED PIPE RUNNER SIZES	
							SINGLE PIPE	MULTIPLE PIPE	NOMINAL DIA.	O. D.
12"	2"	16"	16"	0.07 CIRC.	6:1	4'-0"	No	Yes, for >2 pipes	3" STD	3.068"
15"	2 1/4"	19 1/2"	19"	0.07 CIRC.	6:1	5'-8"	No	Yes, for >2 pipes	3" STD	3.068"
18"	2 1/2"	23"	21 1/2"	0.07 CIRC.	6:1	7'-3"	No	Yes, for >2 pipes	3" STD	3.068"
24"	3"	30"	27"	0.07 CIRC.	6:1	10'-6"	No	Yes, for >2 pipes	3" STD	3.068"
30"	3 1/2"	37"	31"	0.18 CIRC.	6:1	12'-1"	No	Yes	4" STD	4.026"
36"	4"	44"	36"	0.19 ELIP.	6:1	15'-4"	Yes	Yes	4" STD	4.026"
42"	4 1/2"	51"	41 1/2"	0.23 ELIP.	6:1	18'-7"	Yes	Yes	4" STD	4.026"

- Slope as shown elsewhere in the plans. Slope of 6:1 or flatter is required for vehicle safety.
- Cement stabilized bedding and backfill shall be in accordance with the Item, "Excavation and Backfill for Structures". Bedding and backfill shall be considered subsidiary to the Item "Safety End Treatment". When concrete riprap is specified around the Safety End Treatment backfill shall be as directed by Engineer.
- The top 4" of void between Precast End Treatments shall be filled with concrete Riprap and shall be considered subsidiary to Safety End Treatment.
- Clear distance between pipes shall be adjusted to provide for the minimum distance between safety end treatments.

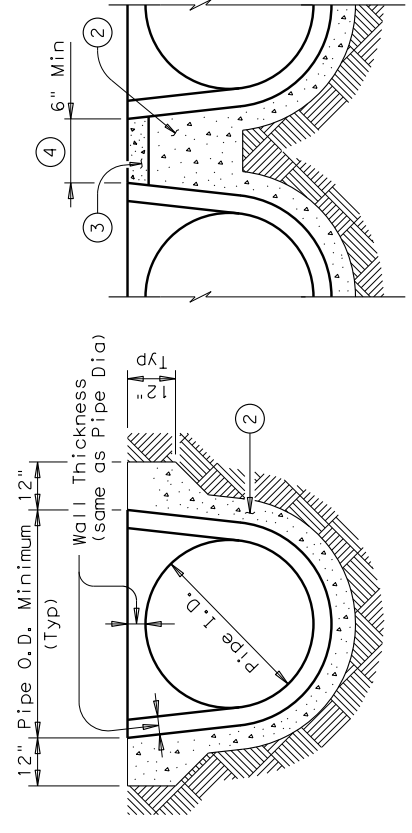


PLAN VIEW - 12" THRU 24"



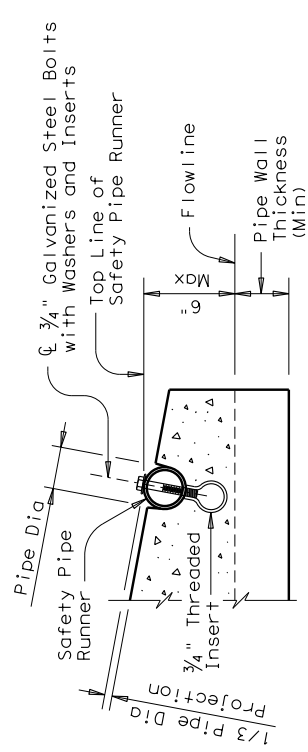
INSTALLATION DETAIL FOR SAFETY PIPE RUNNERS
(If required)

LONGITUDINAL ELEVATION - 12" THRU 24"

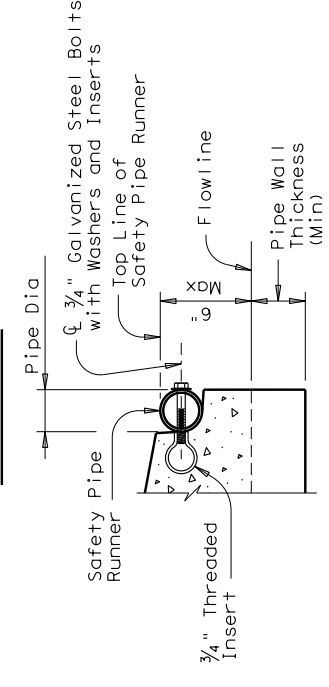


SECTION A-A MULTIPLE PIPE INSTALLATION

OPTION A



OPTION B



END DETAILS FOR INSTALLATION OF SAFETY PIPE RUNNERS
(If required)

GENERAL NOTES:
 Precast safety end treatment for reinforced concrete pipe may be used for TYPE II end treatment as specified in Item "Safety End Treatment". When Precast Safety End Treatment is used as a Contractor's alternate to mitered RCP, Riprap will not be required unless noted otherwise on the plans.
 Synthetic fibers listed on the "Fibers for Concrete" Material Producer List (MPL) may be used in lieu of steel reinforcing in riprap concrete unless noted otherwise.
 All precast concrete end sections shall be manufactured in accordance with Item "Reinforced Concrete Pipe" and in accordance with ASTM Specification C-76, Class III, Wall B for circular pipe.
 Precast concrete end sections shall be provided with a spigot or bell end for compatibility to upstream or downstream end conditions with sufficient annular space to allow for mortar, cold applied asphalt joint compound or pre-formed plastic gasket material.
 Methods of lifting shall be provided by the manufacturer for ease of loading, unloading and installation.
 Pipe Runners are designed for a traversing load of 10,000 lbs at yield as recommended by Research Report 280-2F, "Safety Treatment of Roadside Parallel-Drainage Structures", Texas Transportation Institute, March 1981.
 Pipe Runners shall conform to the requirements of ASTM A53 (Type E or S, Grade B), ASTM A500 (Grade B), or API 5LX52.
 All steel components except reinforcing, shall be galvanized after fabrication. Galvanizing damaged during transport or construction shall be repaired in accordance with the specifications.

Texas Department of Transportation
Bridge Division Standard

PRECAST SAFETY END TREATMENT
TYPE II ~ PARALLEL DRAINAGE

PSET-RP

FILE: psetrpss.dgn
 DN: RLW CK: KLR DW: JTR CK: GAF
 CONT: SECT JOB HIGHWAY
 February 2010
 REVISIONS
 11-10: Add note for synthetic fibers.
 COUNTY SHEET NO.

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.

PIPE I.D.	RCP WALL THICKNESS	TP WALL THICKNESS	"D" (1)	MAXIMUM SLOPE	MINIMUM LENGTH OF UNIT	PIPE RUNNERS REQUIRED		REQUIRED PIPE RUNNER SIZES	
						SINGLE PIPE	MULTIPLE PIPE	NOMINAL DIA.	O.D.
12"	2"	1.15"	17"	6:1	4'-9"	No	Yes, for >2 pipes	3" STD	3.068"
15"	2.25"	1.30"	20.50"	6:1	6'-5"	No	Yes, for >2 pipes	3" STD	3.068"
18"	2.50"	1.60"	24"	6:1	8'-0"	No	Yes, for >2 pipes	3" STD	3.068"
24"	3"	1.95"	31"	6:1	11'-3"	No	Yes, for >2 pipes	3" STD	3.068"
30"	3.50"	2.65"	38.50"	6:1	14'-8"	No	Yes	4" STD	4.026"
36"	4"	2.75"	45.50"	6:1	17'-11"	Yes	Yes	4" STD	4.026"
42"	4.50"	N/A	52.50"	6:1	21'-2"	Yes	Yes	4" STD	4.026"

- Dimension "D" is based on Reinforced Concrete Pipe (RCP) meeting the requirements of ASTM C-76, Class III, (RCP Wall "B" thickness). Adjust "D" for any other wall thickness used. For Thermoplastic Pipe (TP) take into account the annular space requirements for grouted connections.
- Slope as shown elsewhere in the plans. Slope of 6:1 or flatter is required for vehicle safety.
- Toewall to be used only when dimension is shown elsewhere in the plans.
- Fill the top 4" of void between precast end treatments with concrete riprap. Concrete riprap is considered subsidiary to the Item "Safety End Treatment".
- Adjust clear distance between pipes to provide for the minimum distance between safety end treatments.
- Provide cement stabilized bedding and backfill in accordance with the Item, "Excavation and Backfill for Structures". Bedding and backfill is considered subsidiary to the Item "Safety End Treatment". When concrete riprap is specified around the safety end treatment, backfill as directed by Engineer.
- Thermoplastic pipe wall thickness may vary. Adjust accordingly. Thermoplastic pipe requires the safety end treatments to have a bell end for grouted connections.

GENERAL NOTES:
 Precast safety end treatment for reinforced concrete pipe (RCP), and thermoplastic pipe (TP) may be used for TYPE II end treatment as specified in Item "Safety End Treatment".
 When precast safety end treatment is used as a Contractor's alternate to mitered RCP, riprap will not be required unless noted otherwise on the plans.
 Synthetic fibers listed on the "Fibers for Concrete" Material Producer List (MPL) may be used in lieu of steel reinforcing in riprap concrete unless noted otherwise.
 Manufacture this product in accordance with Item "Safety End Treatment" except as noted below:
 A. Provide minimum reinforcing of #4 at 6" (Grade 40) or #4 at 9" (Grade 60) each way or 6"x6" - D12 x D12 or 5"x5" - D10 x D10 welded wire reinforcement (WWR).
 B. For precast (steel formed) sections, provide Class "C" concrete (f'c = 3,600 psi).

At the option and expense of the Contractor the next larger size of safety end treatment may be furnished; as long as the "D" dimension cast is that of the required size of pipe.
 Pipe runners are designed for a traversing load of 10,000 Lbs at yield as recommended by Research Report 280-2F, "Safety Treatment of Roadside Parallel-Drainage Structures", Texas Transportation Institute, March 1981. Provide pipe runners meeting the requirements of ASTM A53 (Type E or S, Grade B), ASTM A500 (Grade B), or API 5LX52.
 Galvanize all steel components except reinforcing steel after fabrication. Repair galvanizing damaged during transport or construction in accordance with the specifications.
 Connect RCP using the Optional Joint for RCP detail shown or in accordance with Item 464 "Reinforced Concrete Pipe". Connect TP by grouting. See PBGC standard for grouted connections with TP and precast safety end treatment.

Texas Department of Transportation

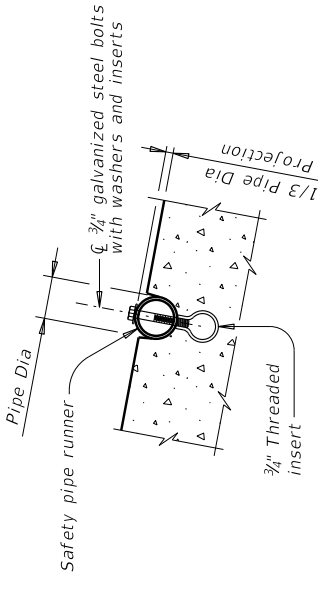
Bridge Division Standard

PRECAST SAFETY END TREATMENT

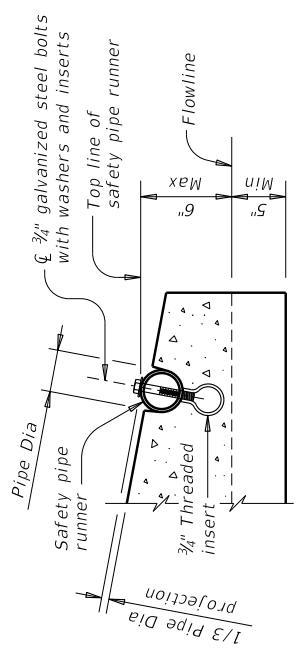
TYPE II ~ PARALLEL DRAINAGE

PSET-SP

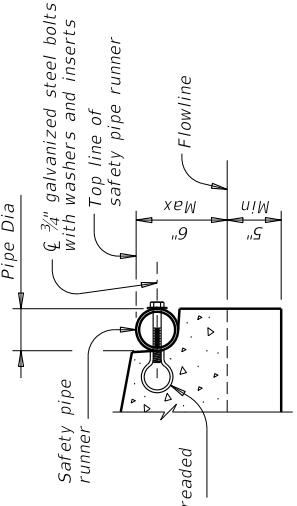
FILE: pset5ps-18.dgn	DN: RLW	CK: KLR	DW: JTR	CK: GAF
CDOTDOT February 2010	CONT: SECT	JOB: HIGHWAY		
REVISIONS				
11-10: Add note for synthetic fibers.	DIST:	COUNTY:		
09-18: Added Thermoplastic Pipe in table.				



INSTALLATION DETAIL FOR SAFETY PIPE RUNNERS
(If required)

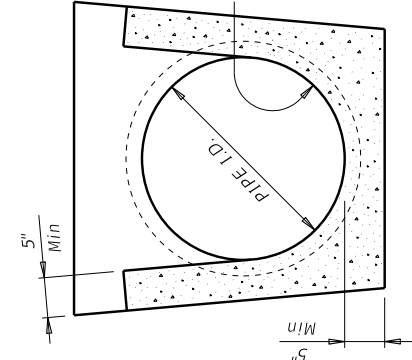


OPTION A

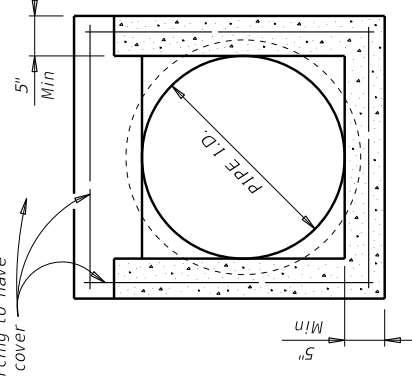


OPTION B

END DETAILS FOR INSTALLATION OF SAFETY PIPE RUNNERS
(If required)

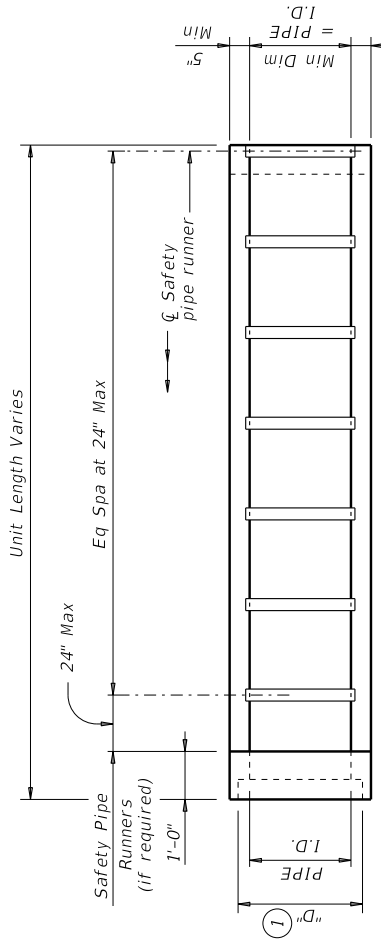


OPTION WITH INVERT BOTTOM



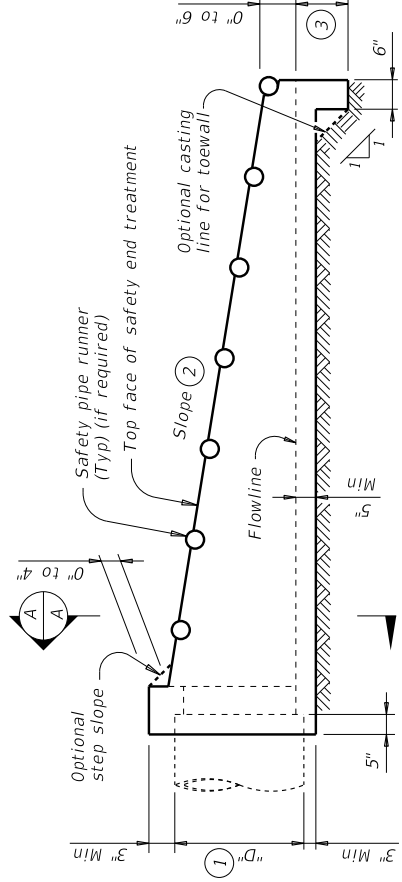
OPTION WITH SQUARE BOTTOM SECTION A-A

SECTION A-A

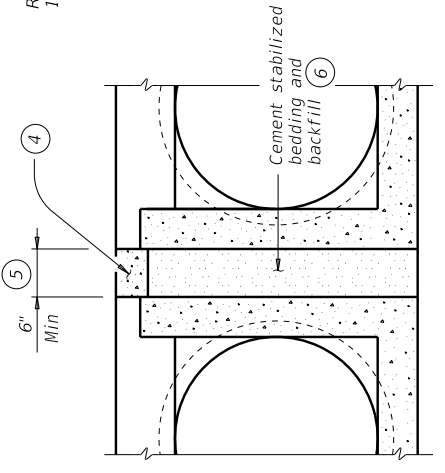


PLAN

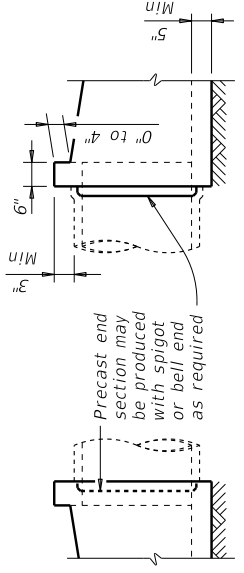
(Showing bell end connection)



LONGITUDINAL ELEVATION
(Showing bell end connection)



MULTIPLE PIPE INSTALLATION



OPTIONAL JOINT FOR RCP
(Showing joint between RCP and precast safety end treatment)

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.

CROSS PIPE LENGTHS, REQUIRED PIPE SIZES, & RIPRAP QUANTITIES (2)						
Nominal Culvert I.D.	Conc Riprap (CY) (6)	Pipe Culvert Spa ~ G	Multi-Barrel ~ Q1	Q2	Conditions of use for Cross Pipes	Cross Pipe Size
12"	0.6	9"	N/A	2'-1"	1'-9"	3" Std (3,500" O.D.)
15"	0.7	11"	N/A	2'-5"	2'-2"	
18"	0.8	1'-2"	N/A	2'-10"	2'-8"	
21"	0.9	1'-4"	N/A	3'-2"	3'-1"	3 1/2" Std (4,000" O.D.)
24"	0.9	1'-7"	N/A	3'-6"	3'-1"	
27"	1.0	1'-8"	N/A	3'-10"	3'-11"	
30"	1.1	1'-10"	N/A	4'-2"	4'-4"	4" Std (4,500" O.D.)
33"	1.2	1'-11"	4'-2"	4'-5"	4'-8"	
36"	1.3	2'-1"	4'-5"	4'-9"	5'-1"	
42"	1.5	2'-4"	4'-11"	5'-5"	5'-10"	5" Std (5,563" O.D.)
48"	1.7	2'-7"	5'-5"	6'-0"	6'-7"	
54"	2.0	3'-0"	5'-11"	6'-9"	7'-6"	
60"	2.2	3'-3"	6'-5"	7'-4"	8'-3"	All Pipe Culverts
66"	2.4	3'-3"	6'-11"	7'-10"	8'-9"	
72"	2.7	3'-4"	7'-5"	8'-5"	9'-4"	All Pipe Culverts

- The proper installation of the first Cross Pipe is critical for vehicle safety. The top of the first Cross Pipe must be placed at no more than 6" above the flow line.
- Size of Cross Pipes, except the first bottom pipe, shall be as shown in the PIPE SIZE table. The first bottom pipe shall be 3 1/2" Standard Pipe (4" O.D.).
- The third Cross Pipe from the bottom of the Culvert shall always be installed using a bolted connection. Care shall be taken to ensure that Riprap concrete does not flow into the Cross Pipe so as to permit disassembly of the bolted connection to allow cleanout access. At the Contractor's option, all other Cross Pipes may also be installed using the bolted connection details.
- Match Cross Slope as shown elsewhere in the plans. Cross Slope of 6:1 or flatter is required for vehicle safety.
- Riprap placed beyond the limits shown will be paid as Concrete Riprap in accordance with Item 432, "Riprap".
- Quantities shown are for one end of one reinforced Concrete Pipe Culvert. For multiple pipe culverts or for Corrugated Metal Pipe Culverts, quantities will need to be adjusted. Riprap quantities are for Contractor's information only.

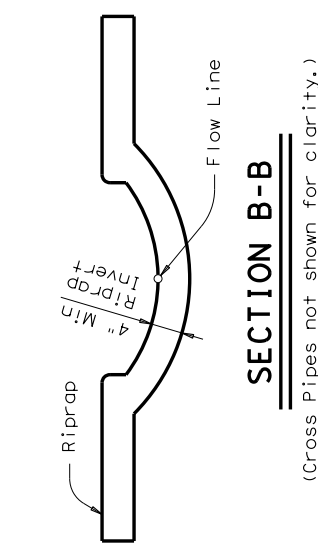
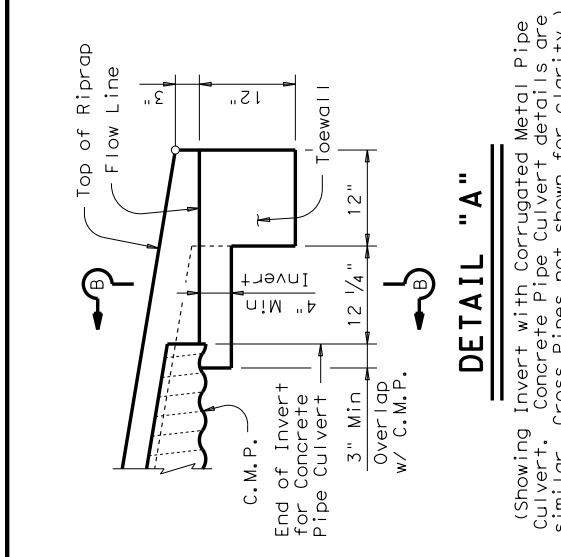
GENERAL NOTES:
 Cross Pipes are designed for a traversing load of 10,000 pounds at yield as recommended by Research Report 280-2F, "Safety Treatment of Roadside Parallel-Drainage Structures", Texas Transportation Institute, March 1981.
 Safety End Treatments shown herein are intended for use in those installations where out of control vehicles are likely to traverse the openings approximately perpendicular to the Cross Pipes.
 Riprap and all necessary inverts shall be Concrete Riprap conforming to the requirements of Item 432, "Riprap". Synthetic fibers listed on the "Fibers for Concrete" Material Producer List (MPL) may be used in lieu of steel reinforcing in riprap concrete unless noted otherwise.
 Payment for riprap and toewall is included in the Price Bid for each Safety End Treatment.
 Cross Pipes shall conform to the requirements of ASTM A53 (Type E or S, Grade B), ASTM A500 (Grade B), or API 5LX52. Bolts and nuts shall conform to ASTM A307.
 All steel components, except concrete reinforcing, shall be galvanized after fabrication. Galvanizing damaged during transport or construction shall be repaired in accordance with the specifications.

Texas Department of Transportation
Bridge Division Standard

SAFETY END TREATMENT
 FOR 12" DIA TO 72" DIA
 PIPE CULVERTS
 TYPE II ~ PARALLEL DRAINAGE

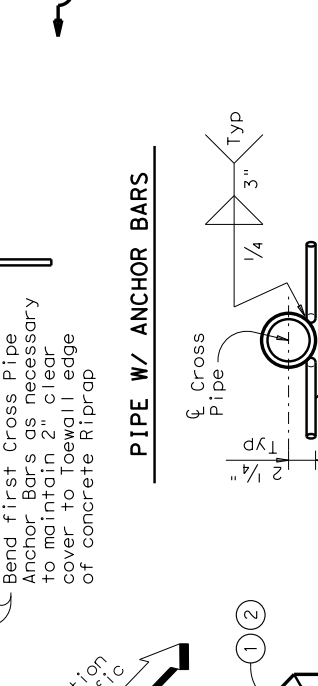
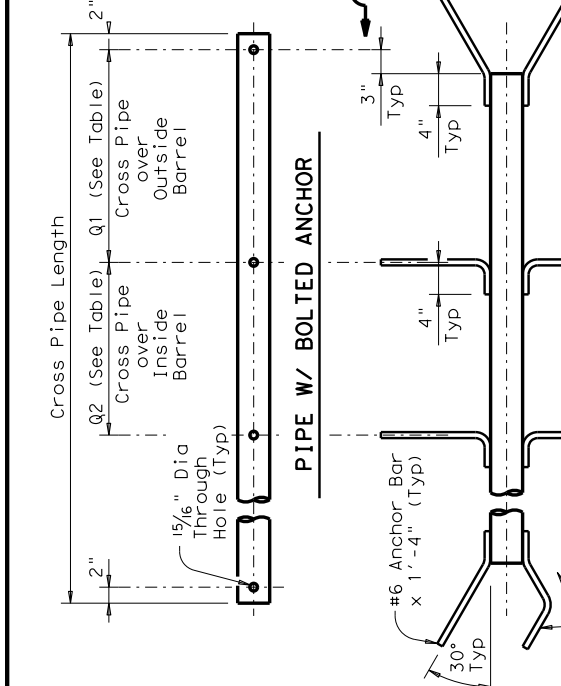
SETP-PD

FILE: setpdpd.dgn	CK: GAF	DW: JRP	CK: GAF
DATE: February 2010	COM: SECT	JOB	HIGHWAY
REV: 11-10: Add note for synthetic fibers.	DIST	COUNTY	SHEET NO.



DETAIL "A"
 (Showing Invert with Corrugated Metal Pipe Culvert. Concrete Pipe Culvert details are similar. Cross Pipes not shown for clarity.)

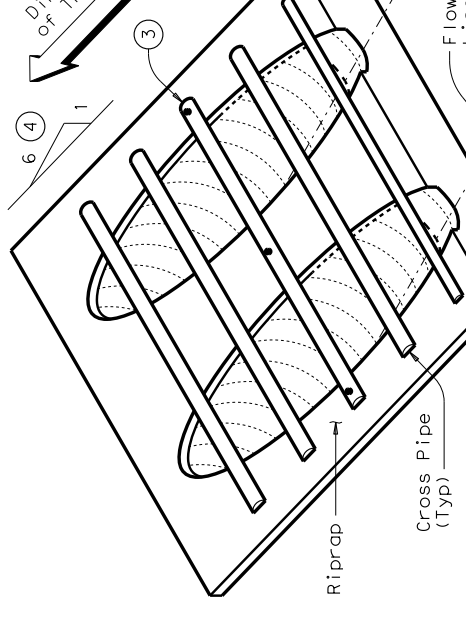
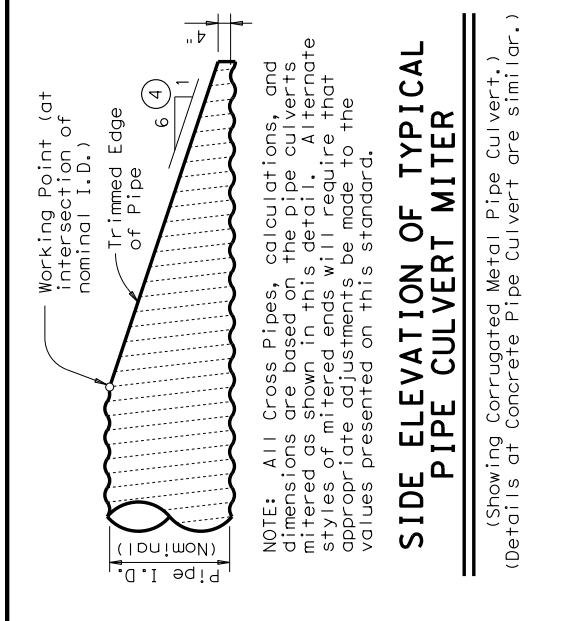
SECTION B-B
 (Cross Pipes not shown for clarity.)



PIPE W/ BOLTED ANCHOR
 Bend first Cross Pipe Anchor Bars as necessary to maintain 2" clear cover to toewall edge of concrete Riprap

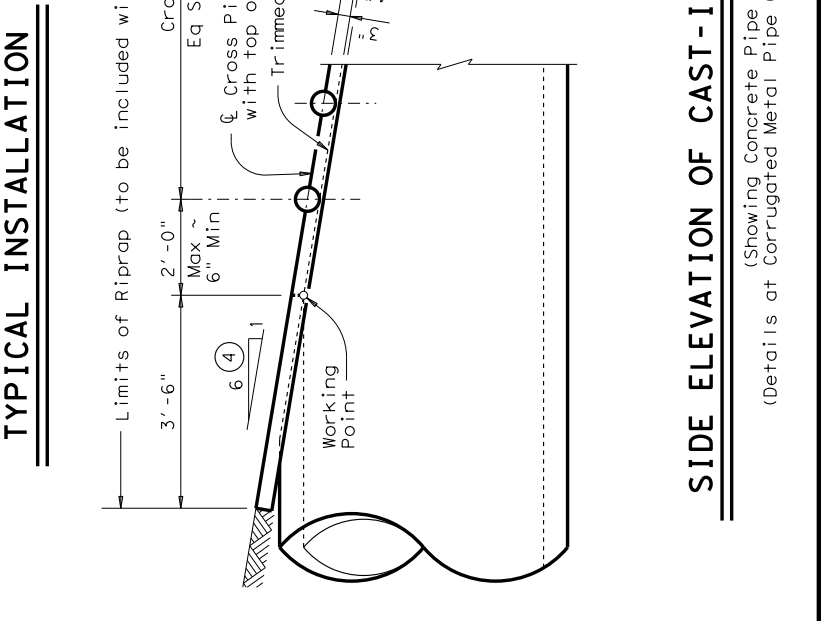
PIPE W/ ANCHOR BARS

SECTION C-C



SIDE ELEVATION OF TYPICAL PIPE CULVERT MITER
 (Showing Corrugated Metal Pipe Culvert.)
 (Details at Concrete Pipe Culvert are similar.)

ISOMETRIC VIEW OF TYPICAL INSTALLATION



SIDE ELEVATION OF CAST-IN-PLACE CONCRETE
 (Showing Concrete Pipe Culvert.)
 (Details at Corrugated Metal Pipe Culvert are similar.)

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Browsers (for SENDERS):	Internet Explorer 6.0? or above
Browsers (for SIGNERS):	Internet Explorer 6.0?, Mozilla FireFox 1.0, NetScape 7.2 (or above)
Email:	Access to a valid email account
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Enabled Security Settings:	<ul style="list-style-type: none"> •Allow per session cookies •Users accessing the internet behind a Proxy Server must enable HTTP 1.1 settings via proxy connection

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